



PERMIT APPLICATION WALKTHROUGH



May 2025

Hello. Welcome to this walk through of the Bridge Permit Application Template. The purpose is to promote familiarity with the document, its required content, and provide an opportunity for questions. If you have any questions, please raise them. For those online, I encourage you to use the react emojis. And beware, later in the presentation I may be asking for a little audience participation.



Agenda

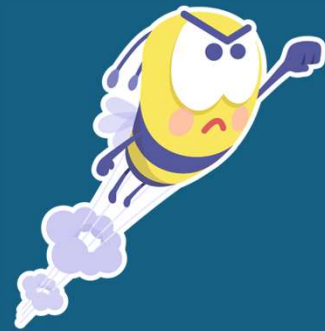
Where to find the application

Application basics

Admin and Navigation Topics

Environmental Topics

Wrap up



For the agenda, we'll review where to find the application template. Then go through the administrivia and navigation topics. Followed by a quick breath before the environmental. Then the wrap up.

Where to find the application

The screenshot shows the official website of the United States Coast Guard Office of Bridge Programs. The left-hand navigation menu is visible, with 'BRIDGE PERMITS' highlighted and circled in red. The main content area features the 'OFFICE OF BRIDGE PROGRAMS' title and a list of links. The link 'BPAG Applicant Template' is circled in red. A QR code is located to the right of the links. The URL 'https://www.dco.uscg.mil/Office-of-Bridge-Programs/' is displayed at the bottom of the page.

<https://www.dco.uscg.mil/Office-of-Bridge-Programs/>

The application template can be found on the Coast Guard Office of Bridge Program's website. To find the site you can use the search terms Coast Guard Bridge Program or Permit in any browser or use the QR code displayed here. Once you're on the main page, select Bridge Permits from the left menu. On the Bridge Permits page, scroll down to the BPAG Applicant Template. BPAG stands for the Bridge Program Application Guide, which is the document that describes the overall bridge permitting process. It's a good document to review if you are contemplating a bridge permit, or if you'd like a refresher during the permitting process.

Clicking on the BPAG Applicant Template will allow you to download a fillable word file. As a best practice, we recommend starting each permit project with a freshly downloaded template.

After filling in the application template, feel free to convert it to a .pdf file and then return it to the District Bridge Office via email, ideally with associated support documents that are logically named. Please note that it is also okay to submit an incomplete application for feedback—that is before receiving all the final supporting documentation. We understand that some permits/approvals/or consultations like water quality certifications, take permits, or biological opinions may take longer to process. Providing the application with most of the information filled in gives the District Office the opportunity to begin processing the

case and prepping the public notice. And don't worry, we'll let you know what's missing before we can deem the application complete. Once all information and support documents are provided, we'll then deem the application complete and do our best to finalize a permit decision within 90 days.

If you have support documents that are too large to email, please contact your USCG District Bridge Office for an alternative method. District offices can coordinate large file transfer via the DOD SAFE.

		<p>APPENDIX B: BRIDGE PERMIT APPLICATION</p> <p>A. THE APPLICATION PACKAGE</p> <p>The application package consists of the following information. Submit information in the format outlined below. If any section is not applicable to the project, state why it is not applicable. This BPAG Applicant Template is also provided on the Bridge Program public website: https://www.dco.uscg.mil/Our-Organization/Assistant-Commandant-for-Prevention-Policy-CG-5P/Marine-Transportation-Systems-CG-5PW/Office-of-Bridge-Programs/Bridge-Permit-Application-Process/.</p> <p>1. Per 33 CFR § 115.50(j), submit application materials to the Coast Guard District Bridge Office that has jurisdiction over the area of the proposed bridge site.</p> <p>Application Date:</p> <p>3/26/2025</p> <p>a. Applicant information:</p> <p>1) Name (company/agency and POC name):</p> <p>Greatest State Dept of Transportation, Project lead: Imma Coolleader</p> <p>2) Address:</p> <p>12345 Somecool Street, Bigtown, Greatest State, 11111</p> <p>3) Telephone number; and</p> <p>202-555-1234</p> <p>4) Email address:</p> <p>Imma.Coolleader@adot.gov</p>
<p>The application... First, the applicant</p>		

Digging into the application... It is the primary means for providing the Coast Guard information on the who, what, where, when, and how that we need to support a Coast Guard bridge permit decision. It also helps us confirm compliance with NEPA and other environmental laws, regulations, and orders. These days, that's a shifting landscape, so maintain good contact with your CG District Bridge Office POC.

The application template is a locked word document with text boxes and x boxes for info. When I say x boxes, I'm sadly not referring to the gaming system. We get that each case is unique and this application is designed to accommodate as many variations as possible. When filling out the application, please provide as much information as possible. Clear responses are appreciated.

For the purposes of this walkthrough, I added fictitious entries inspired by multiple cases. In other words, this does not represent an application for a single bridge project, but rather it's a conglomerate of examples. We roughly organized the application by the Who, What, Where, When, How, and Why not give us a lot more.

Start the document with a date, which may be added immediately prior to submission. Then enter who the applicant is, including both the agency or organization plus the lead person's name and contact information.

Next, Consultant



b. Consultant/Agent information (if employed):

Check here if not applicable and leave this section blank: ☐

1) Name (company/agency and POC name):

Best Bridge Builders, POC: Reilly D. Best

2) Address:

9876 Awesome Ave, Smallertown, Greatest State, 11111

3) Telephone number:

202-555-9876

4) Email address; and

RDBest@BBB.eng


5) Document authorizing the consultant/agent to obtain permits on behalf of the applicant:

Cite enclosure(s) in the application package, list title and date of document(s), as appropriate:

BBB Authorization Letter, 31 June 2024. Encl-1_BBB_authorization_31Jun2024.

If there is a consultant working on behalf of the permit applicant, we also request their information, as well as a letter or email authorizing that consultant to work on behalf of the applicant.

We understand that bridge projects can span years. If anything changes with the points of contact, the schedule or related documents, please provide the updated info to your Coast Guard Bridge Office point of contact.

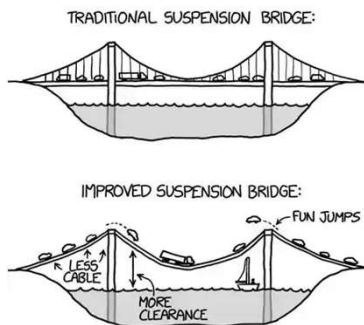
		<p>c. Name of Proposed Bridge(s) (must be consistent with the plan sheet title block):</p> <p>Honorary Modern Person (State Route 111) Bridge, No. 333</p> <p>1) Name of the waterway that the bridge(s) is located in or over:</p> <p>Special River or Ye Olde Bay or Mighty Creek Canal</p> <p>2) Number of miles above the mouth of the waterway where the bridge(s) would be located and provide latitude and longitude coordinates (to five decimal places) at centerline of navigation channel (Lat/Long must be determined using WGS-84 datum. Contact the local Coast Guard Bridge Office for guidance if needed):</p> <p>Milepoint:</p> <p>mile 3.33</p> <p>[Note: measured in statute or road miles]</p> <p>Lat/Long:</p> <p>38.868481, -77.005631</p> <p>3) City or town, county/parish, and state where the bridge(s) would be located at, near, or between:</p> <p>Between ThisCity, County A, ST and ThatTown, County B, ST</p> <p>[Note: use "at" when it is in one city/town; use "near" to name the nearest city/town; use "between" when it goes between two municipalities.]</p>
	<p>What, Where</p>	

With the who established, we then ask for the what and where. What is the name of the proposed bridge, the waterway that it crosses or is over, the mile point of that waterway, and the position of the center of the structure. The mile point, or river mile, is measured from the mouth of the waterway to the center of the bridge and is in statute or road miles, not nautical miles. Please contact the District Office if you have questions about how to determine the mile point or to verify it for existing bridges.

Please provide the latitude and longitude for the center of the structure in decimal degrees out to five decimal places. Note that WGS84 horizontal datum is the preferred reference coordinate system for latitude.

Then further clarify where the bridge is by listing the city or cities the bridge is at, near or between and the associated counties or parishes and states. Be as specific as possible with these pieces of information. It's critical that the name, mile point, and latitude/longitude position, be correct and consistent throughout all the documents. We will make sure they align with what is in the plan sheet title blocks and on the Final Bridge Permit. Precise location and mile point is especially important when there are multiple bridges in a row on the same waterway.

What's going on?



- 4) Brief description of project to include action being taken, type of bridge(s) proposed [fixed or movable (drawbridge, bascule, vertical lift, swing span, pontoon, etc.), highway, railway, pedestrian, pipeline] and existing bridge(s) at project site, if applicable:

The purpose of this project is to replace the existing double bascule highway bridge with a fixed three span high-level suspension bridge. The existing bridge is in poor condition and requires ever increasing routine maintenance to remain operational. The replacement will more safely support increasing numbers of vehicle, pedestrian and maritime traffic. The proposed bridge will include four travel lanes, two shoulders, and a bike/pedestrian path.

- 5) Is this project a design-build or alternate design project:

☒ Yes ☐ No

If yes, provide a brief description:

This is a design-build project with the initial contract issued on 29 Feb 2023. Contract will be let in May 2025. Design will be completed in phase one, construction in phases two and three, and demolition in phase four.



- 6) Date of plans and number of plan sheets (i.e., 1 of 4, indicate if revised, include multiple dates when necessary, etc.):

4 sheets dated 29 Feb 2024, Encl-2_CG_Plans_29Feb2024.

The next requirement is a description of the project or more information on the what. What is it that is being built, modified, and/or demolished. Is it a bridge for a highway, railroad, mixed use path, pedestrian trail, pipeline, zipline or causeway? Is the bridge fixed in place or is it a drawbridge that opens to let boats through? If it opens, is it a bascule, vertical lift or swing bridge? Generally speaking, why does the structure need to be built or replaced and what form will it take?

Next, note if it is a design build or alternate design project and describe what that means in terms of contract letting, responsibilities and phasing. If it is a design build project, we will include specific permit conditions related to that and the need to receive as built schematics.


Following that...What is the date of the plan sheets and how many are there? Plan sheets are important because they provide a visual representation of the bridge's characteristics—particularly those that are most important to the maritime community. And be advised that these plans will accompany the permit, which is valid for the full life of the bridge. We have a different presentation that covers plan sheet requirements in depth.

Estimated Cost and Funding Sources	
 	<p>7) Provide the estimated cost of the bridge(s) and approaches with proposed vertical and horizontal navigational clearances:</p> <p>\$123.45M</p> <p>8) Identify the type(s) and source(s) of project funding (federal, state, private, etc.):</p> <p>60% Federal Highway Administration; 40% Great State</p> <p>9) Describe the proposed project timeline (from permit to construction completion, e.g., NEPA scoping, NEPA document completion, request for bids, designs complete, build starts, construction complete, etc.):</p> <p>Construction is anticipated to take 3 years including demolition. Phase one (Design) – Jun 2025 – Jun 2026 Phase two (Construction of new piers) – Jul 2026 – Dec 2027 Phase three (Construction of deck) – Dec 2027 – Feb 2028 Phase four (Demolition) – Feb 2028 – May 2028</p>

Still on the topic of what... What is the estimated cost of the bridge? We keep track of bridge costs for reporting purposes.

The next request is for the funding source--where do project funds come from? List the local, state, and/or federal sources. We ask for this partly because it clues us in to who will be the lead federal agency for the purposes of ensuring its conformity with applicable environmental requirements. If another federal agency is funding the project, the Coast Guard will generally not act as the lead federal agency. If there is no federal money being spent, however, then the Coast Guard may act as the lead federal agency for completing NEPA and NHPA Section 106 requirements.

Then we move to when and ask for the proposed timeline. When are pivotal points estimated to begin and end? How long will it take to complete NEPA, complete the design, request bids, award the contract, initiate and complete construction, open the bridge to traffic, remove existing structures? Please describe the major components and timing of each phase.

	<div data-bbox="383 308 1369 722" style="border: 1px solid black; padding: 10px;"> <p>10) Identify any other Federal actions (e.g., permits, permissions, approvals, or consultations, etc.) and the agency associated with the proposal:</p> <p>CWA Section 404 permit and 408 permission, U.S. Corps of Engineers; Endangered Species Act, USFWS, NMFS; Migratory Bird and Essential Fish Habitat Consultation; National Historic Preservation Act Programmatic Agreement</p> <p>11) Identify any other non-Federal agency actions and the agency (e.g., permits, permissions, approvals, or consultations) associated with the proposal:</p> <p>Water Quality Certification, State Dept of Environment; Coastal Zone Management Consistency Determination</p> </div>
	<h2 style="margin: 0;">Other Federal and non-Federal Agencies & Actions</h2>

Touching once more on who and what else... Please provide a list of other involved agencies and required agencies. Some Federal agency actions might include species take permits, Section 106 adverse impact determinations for Preservation of Historic Properties, and a Section 404 permit and/or Section 408 permission from the US Army Corps. Then note the non-Federal or state agencies and actions such as Clean Water Act Certifying Authorities and Water Quality Certifications, State Coastal Zone Concurrence, etc. Basically, what other agencies are involved and what actions are required?

Legal Authority



d. Legal authority for proposed action:

- 1) Is the applicant a state or municipal agency with eminent domain authority over private, state, and/or local property? (If yes, the primary authority will be presumed without proof)

☒ Yes ☐ No ☐ N/A

If yes, please identify what agency has eminent domain authority:

Greatest State Department of Transportation

- 2) If there is an existing bridge(s) being replaced or modified, and the applicant does not own it, include a signed statement from the bridge owner authorizing the removal or modification work.


☐ N/A, applicant owns the existing bridge

Cite enclosure(s) in the application package, list title and date of document(s), as appropriate:


Existing bridge to be replaced is owned by Civic Cooperative Corps. Letter of authorization issued on 23 Nov 2023, Encl-3_CCC_Authorization_23Nov2023.

Next, we walk through a few questions to confirm legal authority. First, is the applicant a state or municipal agency with eminent domain authority over the property where the project is located? If so, great, mark yes and keep going. We'll presume that all is good and not ask for further proof because we assume legitimate ownership by municipal, state and federal agencies. If the applicant is not an agency or municipality, mark No and also keep going. If this or other questions are not applicable or do not apply, mark N/A and convey why.

If there is an existing bridge being replaced or modified and the applicant **does not** own it, we ask for an authorization statement from the bridge owner. Please share the file name and document date in the space provided and send the document electronically. The mantra to keep in mind throughout is: fill in the file name and date then deliver the document.

	<h2 style="text-align: center;">Privately Owned Bridges or Project Property</h2>
	<div style="border: 1px solid black; padding: 10px;"> <p>3) For privately owned bridges, state whether the applicant has the right to build in accordance with 33 CFR § 115.05. If the applicant does not own the property needed to build the bridge(s) as proposed, include a signed statement (e.g., deed or easement) from the property owner or owners authorizing the proposed construction or modification work.</p> <p> <input checked="" type="checkbox"/> N/A, publicly owned, not a privately owned bridge <input type="checkbox"/> Privately owned, applicant has right to build </p> <p>Cite enclosure(s) in the application package, list title and date of document(s), as appropriate:</p> <div style="background-color: #f0f0f0; padding: 5px; border: 1px solid #ccc;"> Click here to enter text. </div> </div>

If the property or the bridge is not municipal or agency-owned, we ask for confirmation of ownership via a copy of a deed, easement or signed statement from the owner authorizing the construction, removal or modification. If this item does not apply mark N/A, and if it applies, fill in the file name, date, and deliver the document.

	<p>e. For international bridges (if applicable) check which authority the bridge(s) is being built or modified under:</p> <p><input type="checkbox"/> N/A</p> <p><input checked="" type="checkbox"/> The International Bridge Act of 1972</p> <p>The Coast Guard requires Presidential approval, via the State Department, before issuing a bridge permit under the International Bridge Act of 1972. Include a copy with the bridge permit application as appropriate. Cite provided enclosure title and date of document:</p> <p>International Joint Commission Decision – received 6 December 2022, Encl-4_International_Joint_Commission_Decision_6Dec2022 Department of State confirmation no Presidential Permit required, dated 31 November 2023, Encl-5_DOS_Confirmation_31Nov2023.</p> <p><input type="checkbox"/> Special Act of Congress</p> <p>Cite the appropriate legislative authority:</p> <p>Click here to enter text.</p> <p>NOTE: Please include a copy of State Department approval for international bridges in the application package for a Coast Guard bridge permit.</p>
<h2>International Bridges</h2>	

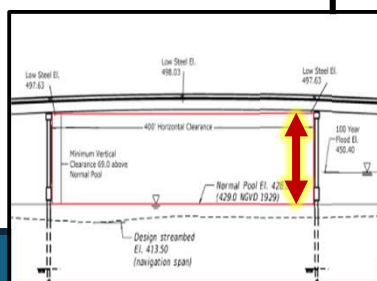
Not surprising, International bridges are a bit special. They fall under the International Bridge Act of 1972 or a Special Act of Congress and also require coordination with the Department of State. If you're dealing with an international bridge project, be sure to fill in the file names, dates, and deliver the documents. If it does not apply, mark N/A. For international bridge projects, be sure to reach out to the CG early in the project such as at or before scoping.

Figure 1 is a detailed bridge layout plan showing the overall structure from New Bedford to NewPort. The plan includes five spans: Span 1 (approach), Span 2 (main arch), Span 3 (approach), Span 4 (approach), and Span 5 (approach). Key features include the main arch, approach spans, and various structural details like piers and abutments. The plan also shows the location of the main arch, approach spans, and various structural details like piers and abutments. The plan is oriented with North at the top, and the distance from New Bedford to NewPort is indicated as 1.37 miles.



13

Vertical Clearance – How high above the water?



f. Dimensions of the navigation opening (All navigational clearances should be stated in U.S. linear feet in decimal form (not feet and inches). For international bridges, provide clearances in both linear feet and meters):

- 1) Vertical clearance(s) as indicated on plan sheets (*Note, this is the minimum vertical distance between the lowest part (e.g., member, chord, or steel) of the superstructure spanning the navigation channel and the recognized high water elevation (e.g., MHW, OHW, 2% flow line, etc.) at the bridge site. Cite clearances above the appropriate high water elevation. In the case of movable bridges, cite clearances in the open and closed positions. In some situations, vertical clearances should be cited at the margins of the navigation channel, and for a bascule bridge clearances at the tip of the open leaves. Include multiple clearances when appropriate.*

33.3 feet to mean high water

-or for a movable structure-

33.3 feet to mean high water in closed position

133.3 feet to mean high water in open position

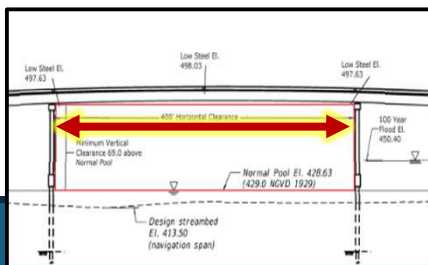
For all the bridge dimensions, please provide them in decimal feet like you see here. We're not engineers or math lovers, so please don't ask us to convert feet and inches to decimal feet.

Okay, first up is vertical clearance or what is the distance between the water level and the bottom of the bridge. This dimension is measured between low steel or low member elevation and mean or ordinary high water. If there is no high water such as between locks, then it may be measured to normal pool elevation or 2% flowline. For movable bridges, be sure to list the vertical clearance in both the open and closed positions as some vessels may need an opening, and some may not. If you have any questions, check with your District Bridge point of contact.

Horizontal Clearance – How wide is the opening?

2) Horizontal clearance(s) as indicated on plan sheets (*Note, this is the horizontal distance, measured normal to the axis (centerline) of the channel, through which the stated vertical clearance is available. Clearance(s) may be between piers (full width of the span), between the bridge protective system, within the margins of the navigational channel, or bank-to-bank in the case of a bridge having no piers or bridge pier protective system within the waterway. Also, list both clearances if there is a difference in the distance between piers and the distance measured normal to the axis of the channel*):

222.2 feet normal to the axis of the navigational channel



Next is horizontal clearance. This dimension is typically measure from pier face to pier face or fender to fender, so long as there is reasonable water depth and vertical clearance. For arched bridges, the horizontal clearance may be narrower than the pier-to-pier distance. Also note that the horizontal clearance should be measured normal to the axis of the channel. That is perpendicular to the channel the vessels will transit. Some bridges, such as the one depicted on the lower right, cross a channel at an angle that is not 90 degrees. In that case, the horizontal clearance is perpendicular to the navigational channel or waterway as represented by the shorter red line.

3) Length of bridge(s) project (Note this is the length of the bridge(s) project as indicated on the plan sheets from abutment-to-abutment or approach-to-approach.):

444.4 feet abutment to abutment

If this is a modification or replacement project, is the length the same as the existing bridge?

☐ N/A, not a modification or replacement

☐ Yes ☒ No

If no, note the difference in length between the existing and proposed bridges.

☐ N/A

The proposed bridge is 44 feet longer than the existing bridge.

4) Width of bridge(s) project (Note this is the width of the bridge(s) at its widest point (out-to-out) as indicated on the plan sheets.):

66.7 feet

If this is a modification or replacement project, is the width the same as the existing bridge?

☐ N/A, not a modification or replacement project

☒ Yes ☐ No

If no, note the difference in width between the existing and proposed bridges.

☐ N/A

The proposed bridge is 11 feet wider than the existing bridge.

Overall Length and Out-To-Out Width



We ask next for the overall length. That is the total length of the bridge from abutment to abutment. If the project is a modification or replacement, is it the same or different from the existing bridge and what's the difference? Follow length with the out-to-out width of the proposed bridge. And if a modification or replacement, will the out-to-out width be the same as the existing bridge or will the proposed bridge be wider or narrower?

Temporary Structures or Bridges



g. Temporary structure(s) or bridge(s). *(Note a temporary work trestle/platform does not span the waterway and is solely used for construction purposes. A temporary bridge will span the waterway, including the navigational channel, and is used for*

transportation or construction purposes). If a temporary structure or bridge will be required, provide the following as applicable:

☐ N/A, no temporary structure or bridge, this section left blank

1) Description of the temporary structure(s):


☐ N/A, no temporary structure

Falsework will be used alongside the shore to construct the new abutments. It will not impact the navigational channel.

-or-

Temporary work trestles extending from the shore alongside both sides of the proposed bridge will be used to support barges and the construction of spans 1, 2, 4, and 5.

Moving on... Will temporary structures, work trestles, platforms, falsework or temporary bridges be used to support construction or demolition or traffic re-routing? Be aware that we recognize a difference between a temporary structure or falsework and a temporary bridge. A temporary work structure does not cross the navigational channel and a bridge completely crosses the navigational channel. If there will be temp structures, describe them in this section and note if they will impede the navigational channel. If it does not apply, mark N/A.

<p>2) Description of the temporary bridge(s):</p> <p><input type="checkbox"/> N/A, no temporary bridge</p> <p>A temporary bridge will be constructed to the north of the existing bridge and south of the proposed bridge. The temporary bridge will support construction and demolition equipment.</p> <p>3) Vertical clearance(s), as indicated on plan sheets <i>(For a temporary structure, only required if it crosses a navigation channel)</i>:</p> <p><input type="checkbox"/> N/A, temporary structure does not cross the navigation channel</p> <p>30.0 feet at the centerline of the navigational channel</p> <p>4) Horizontal clearance(s) as indicated on plan sheets <i>(List both clearances if there is a difference in the distance between piers and the distance measured normal to the axis of the channel. For a temporary structure, only required if it encroaches upon a navigation channel)</i>:</p> <p><input type="checkbox"/> N/A, temporary structure does not encroach on the navigation channel</p> <p>333 feet</p>	
<h2>Temporary Bridge</h2>	<p>5) Length of the proposed temporary structure(s)/bridge(s):</p> <p>444.4 feet</p> <p>6) Width of the proposed temporary structure(s)/bridge(s):</p> <p>55.5 feet</p> <p>7) Identify the schedule and extent of removal(s) for the temporary structure(s)/bridge(s):</p> <p>The temporary bridge is estimated to be in place for six months starting in Jul 2025. It will be removed when the proposed bridge is complete.</p>

For a temporary work bridge, which crosses a waterway, we ask for the same types of dimensions as for the proposed bridge, including vertical and horizontal clearances, overall length, and out-to-out width. Also discuss the schedule and removal of the temporary bridge—when will it be installed, how long will it be in place, when and how much of it will be removed? Temporary bridges are typically authorized in the permit for the proposed permanent structure. If this does not apply, mark N/A.

Existing Bridge(s)

h. Existing bridge(s) at the project site, if applicable:

☐ N/A, no existing bridge, this section left blank.

1) Name of existing bridge(s): (e.g., US 40 Highway Bridge; or Coleman Memorial Bridge; or State Route 7 Bridge also known as Preston Falls Bridge):

Honorary Historic Person (State Route 111) Bridge, No. 332

2) Waterway milepoint (in statute miles):


mile 3.31

3) Type of bridge(s) and description (number of lanes, spans, fixed or moveable (drawbridge, bascule, vertical lift, swing span, pontoon, etc.); highway, railway, pedestrian, pipeline, etc.):

Fixed highway bridge with two travel lanes and narrow shoulders.


-or-

Double leaf bascule bridge with two travel lanes and one sidewalk.




If there is an existing bridge, one that is being replaced or modified, we ask for those details in this section. If it does not apply, mark N/A.

For the existing bridge, provide the name and waterway milepoint or river mile. Then, describe what type of bridge it is. Is it fixed or movable? A highway, rail, pedestrian or other kind of bridge?

	<div data-bbox="308 405 612 449" data-label="Section-Header"> <h2>Existing: Movable</h2> </div>
	<div data-bbox="678 338 1360 919" data-label="Form"> <p>4) For movable spans identify the existing drawbridge operating regulation governing the structure (if applicable):</p> <p><input type="checkbox"/> N/A, fixed bridge</p> <p>a) If the existing bridge(s) has a movable span, identify whether its operating schedule is regulated by 33 CFR § 117.5 or if it operates under a special operating regulation found in 33 CFR Part 117 Subpart B (if so, cite the regulation):</p> <p>33 CFR 117.5</p> <p>b) Modification of an existing drawbridge may require revision or removal of the existing regulation (e.g., if the bridge project involves replacing the existing drawbridge with a fixed bridge). Contact the local Coast Guard District Bridge Office to determine if the existing regulation will transfer to the new bridge, if a new regulation will be proposed, if it will be removed, or if there will be no change required. Identify the anticipated status of the drawbridge regulation (e.g., regulation transferred, new regulation, regulation removed, no regulation change.):</p> <p>No regulation change. -or- Remove regulation because new bridge is fixed. -or- Transfer regulation to proposed bridge.</p> </div>

If the existing bridge is movable, note the governing CFR citation and what will happen to the regulation when the proposed bridge is constructed. Options include no change; remove the regulation because the new bridge is fixed and no longer movable; or transfer the regulation to the proposed bridge. This information will be used to start the regulation transfer or removal process. Mark N/A if this doesn't apply.

		<p>5) Latitude and longitude coordinates (degree/minute/second) at centerline of the existing bridge(s) based on WGS-84 horizontal datum:</p> <p>38.868847, -77.005298</p> <p>6) Dimensions of the existing bridge(s): <i>(The proposed and existing vertical clearances must be compared using the same datums. This may require surveying the existing bridge. All navigational clearances should be stated in U.S. linear decimal feet. In addition, provide clearances in meters if an international bridge(s)):</i></p> <p>a) Vertical clearance(s) as built (include both the open and closed-to-navigation clearances for movable spans). <i>(For modification and replacement projects, the applicant must cite the vertical clearance of the existing bridge and the proposed bridge from the same datum. If the vertical datum for the existing bridge differs from the proposed vertical datum (tidal referenced to geodetic), show all necessary converted vertical clearance values and note the original values in the notes section on the plan sheets to demonstrate any change in approved clearances. If conversions (i.e., Mean Low Tide to Mean Low Water) cannot be made, it is necessary for the applicant to survey the existing bridge to provide as-built clearances using the same verifiable vertical datum (tidal and geodetic) as the proposed project.):</i></p> <p>33.2 feet to mean high water</p> <p>-or for a movable structure-</p> <p>33.2 feet to mean high water in closed position unlimited in open position</p>
	Existing: Characteristics	

For the existing structures, note the centerline latitude and longitude position in decimal degrees out to five spaces. Keep in mind that the dimensions for the existing and proposed structures should be determined using the same datums. Depending on the age of the existing bridge, a survey might be necessary to compare like with like and not broccoli to barbecue. Follow the position with the vertical clearance. If appropriate, include vertical clearance for both open and closed positions.

b) Horizontal clearance as built:

220.2 feet normal to the axis of the channel.

c) Length of existing bridge(s):

440.4 feet

d) Width of existing bridge(s): (This is the width of the bridge(s) at its widest point (out-to-out)):

55.5 feet

7) Owner of the existing bridge(s):

Greatest State Dept of Transportation

8) Previous permit authority (or authorities), date(s) of permit and/or amendments, including issuing agency (cite enclosure(s) when available):

Permit issued 4 May 1937 under authority of an Act of Congress approved 3 march 1899.

9) If available include copies of previous permit(s) and plans with application (cite enclosure when available):

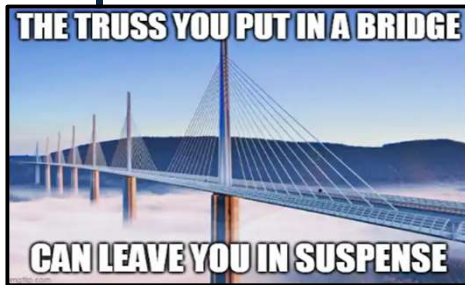
Encl-6_Permit_4May1937; Encl-7_Permit_Amendment_31Jun1957

Existing: Characteristics



Follow the vertical clearance with the horizontal clearance, the overall length, and the out-to-out width. Those should all be in decimal feet. We also ask for the owner of the existing bridge. While it's rare, occasionally the owner of the existing bridge will be different than the owner of the proposed bridge. Next provide the authority under which the existing bridge was permitted as well as a copy of previous permits and plans, if available. List the file names, dates, and deliver the documents.

Construction, Traffic, and Removal



i. Construction methodology, if known, and removal plans for existing bridge(s), as applicable:

1) Discuss proposed construction methodology and restrictions if known:

☐ N/A, construction methodology not known

Construction is anticipated to commence on 31 Nov 2023 and last for three years. Construction of the new proposed bridge will take place in two phases prior to demolition of the existing bridge and parts are not being used in the new build. Phase 1 includes [foundation & pier work description]. Bubble curtains will be used and cofferdams will be placed prior to 1 Apr. Time of year traffic/fish patterns limit the reconstruction to between October through March. Time of year restrictions for fisheries restrict in-water work between March 1 and June 30. Phase 2 includes installing I-beams, pouring the concrete deck, installing railings and street lights.

-or-

The foundations will consist of driven HP16x101 capped with a reinforced concrete cap, poured concrete footings/foundations inside cofferdams. Piles will be driven with a crane positioned on the north side of the channel. No causeway or temporary work structure will be needed. Prefabricated bridge elements are being utilized such as superstructure modules (pre-assembled steel beam units with a precast concrete deck with traffic barrier). A barge mounted crane will be utilized to erect the new superstructure modules. A separate material barge will be utilized for loading/unloading bridge materials and elements.

Now we ask for the how. Please describe the proposed construction methodology for the project and if there are any restrictions. When is construction anticipated to begin? What actions will take place first? When/where will falsework or temporary platforms be placed? Are there any anticipated or required in-water work restrictions such as those to avoid harming fish resources? Will cofferdams be used? Poured concrete? How about barges? The more detail here the better, particularly the how and when of what's anticipated. We're not looking for pages and pages, but rather a clear and concise description that notes the key points, steps, phases and any restrictions. This information also provides us with an idea of the future coordination necessary to ensure safe navigation. Don't be afraid to put more detail than you see in these examples.

2) Discuss maintenance of land traffic during construction activities:

☐ N/A, land traffic maintenance not required

Land traffic will be maintained on the existing bridge until the new bridge is complete. Once bridge construction is complete, traffic will be shifted to the new structure.

-or-

Land traffic will remain on the existing bridge/road, then briefly on a temporary bridge during construction until shifted to the new bridge.

3) Discuss extent of removal of existing bridge(s) (e.g., in its entirety, down to or below the natural bottom of the waterway, to a specific elevation, etc.), including parts in the water and on land (if applicable) and time needed for removal. Cite all correspondence that influenced removal depths:

☐ N/A, no existing bridge

The existing to-be-replaced bridge will be removed in its entirety.

-or-

Piers 3 and 4 located in the navigational channel will be removed to four feet below the mudline. All other portions of the existing to be replaced bridge will be removed to the mud line or natural ground line. Encl-8_USACE_404_permitletter_31Oct2023.



Land Traffic & Extent of Removal

After talking about construction and possible restrictions, please discuss how land traffic will be maintained during the construction activities. Will traffic move to a temporary work bridge? Will one direction of traffic be switched over when half of the proposed bridge has been constructed? Will traffic be detoured to a completely different waterway crossing? We ask this because we often use the bridge's "opening to traffic" as a point of reference for other completion activities. For example, a permit condition may state "the applicant has 90 days after the newly constructed bridge opens for traffic, to remove the old bridge and clear the waterway of construction activities."

And related to permit language, particularly for the removal condition, we need input that discusses the extent of removal of any existing bridge, bridges or parts thereof. We prefer to see bridges removed in their entirety so that there is no chance the foundations become hazards to navigation for transiting vessels. However, we also understand that that is not always feasible. What is important is that the removal elevations are in line with any other permits, certifications, or permissions such as those issued by the Army Corps.

Demolition



4) Discuss demolition methodology:

☐ N/A, no existing bridge

Temporary protective shielding will be installed. A barge mounted crane will be utilized to lift and remove segments of the existing concrete deck and to remove the existing steel beams. A separate material barge will be utilized for unloading the existing bridge materials.

-or-

The causeway of the existing roadway will be removed once the new roadway and bridge are open to the public. As much of the existing roadway and underlying causeway will be removed as possible without resulting in excessive damage to the surrounding wetlands. The causeway will then be backfilled where required such that there are not depression areas lower than the adjacent wetlands. This effort is anticipated to take 6-months and will commence once traffic is diverted onto the new roadways/bridge.

NOTE: In the interest of navigational safety, the Coast Guard must make the final decision concerning the extent of bridge(s) removal.

With the extent of removal established, we then ask for a description of how the demolition will take place. In particular, how will the removal impact the navigational channel? Will there need to be temporary closures? The specific details and necessary notifications will be worked out in the future, but it's good to have an idea of the general extent earlier in the process.

Waterway Data Requirements

- Contact the Coast Guard District Bridge Office during scoping.
- They will determine if a navigation impact report is required and what data should be included.
- The data provided will be used to craft the Preliminary Navigation Clearance Determination (PNCD).
- The PNCD drives the vertical and horizontal clearances.
- See BPAG Appendix A




During scoping, contact the local Coast Guard District Bridge Office to determine if a navigation impact report is required and what data should be included. As discussed in the Navigation Impacts and Clearance Determinations, the data informs the preliminary clearance determination, which conveys the vertical and horizontal clearances likely to receive a permit.



Ok, so having covered the who, what, where, when, and how, brings us to the next section on environmental compliance. Before diving in, are there any other questions? Then take an intentional breath and maybe a sip of coffee/tea/water. And beware, in this next section, I may be asking for input from attendees.

National Environmental Policy Act



The logo features a blue umbrella with the letters 'NEPA' in white. Below the umbrella, a list of laws is provided:

- Clean Air Act
- Environmental Justice Executive Order
- U.S. Department of Transportation Act of 1966; Section 4(f)
- Clean Water Act
- Section 106 of the National Historic Preservation Act
- Endangered Species Act


1. **National Environmental Policy Act** - The National Environmental Policy Act (NEPA) (42 USC 4321, et seq.) requires federal agencies to analyze the impacts of their proposed major federal actions on the human environment before the action is taken by considering the natural and physical environment and the relationship of the people with that environment. Coast Guard bridge permits are major federal actions that require the preparation of an environmental evaluation document describing the potential environmental effects under NEPA.

a. Lead Federal Agency:

Federal Highway Administration

-or-

Greatest State with NEPA Assignment Authority



b. List any Cooperating Agencies for the project:

☐ None

FTA, USCG, USACE, NOAA NMFS, USFWS, DOI, EPA, etc.

-or-

No cooperating agencies because of categorical exclusion.

Jumping in, we'll go through a brief rundown of how to address each environmental requirement that might apply. First things first is the National Environmental Policy Act or NEPA—the umbrella under which many other policies are coordinated. Starting off, we need to know who's acting as the lead federal agency. This ideally will be settled fairly early in the project coordination process. Depending on funding sources, the lead Federal agency could be Federal Highway Administration, Federal Transit Administration, or the Army Corps of Engineers. It might even be a State DOT which has been designated with NEPA assignment authority. Does your state have NEPA assignment authority? We'll also ask you to list the cooperating agencies. More often than not, particularly when we're dealing with a highway bridge, FHWA would be the lead federal agency while some of the usual suspects for cooperating agencies include the Coast Guard, the Army Corps, NOAA, USFWS and so on.

<p>c. Type of environmental document prepared by the Lead Federal Agency (check applicable document):</p> <p><input checked="" type="checkbox"/> Environmental Impact Statement/Record of Decision (EIS/ROD)</p> <p>Cite enclosure(s) in the application package, list title and date of document(s), as appropriate:</p> <p>EIS dated Feb 2017, Encl-9_EIS_Jan2017 ROD dated 30 Feb 2017, Encl-10_FHWA_ROD_30Feb2017.</p> <p><input type="checkbox"/> Environmental Assessment/Finding of No Significant Impact (EA/FONSI)</p> <p>Cite enclosure(s) in the application package, list title and date of document(s), as appropriate:</p> <p>Click here to enter text.</p> <p><input type="checkbox"/> Categorical Exclusion (CE)</p> <p>Cite enclosure(s) in the application package, list title and date of document(s), as appropriate:</p> <p>Click here to enter text.</p>	<div data-bbox="857 352 1442 499">NEPA Level</div> <div data-bbox="1094 533 1208 680"> </div> <div data-bbox="816 716 1430 961"> <p>d. Has the environmental document been modified, reevaluated, supplemented or rescinded for the proposed action?</p> <p><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>If yes, summarize and cite enclosure title(s) in the application package and cite date(s) for all documents as appropriate:</p> <p><input type="checkbox"/> N/A, document has not been modified, reevaluated, supplemented or rescinded</p> <p>EIS Reevaluation completed 31 September 2024, Encl-11_EIS_Reeval_31Sep2024.</p> </div>
EIS, EA, or CE	

We need to know what level of environmental review pertains to the project. Is it going to require a full-blown Environmental Impact Statement or are we looking at an Environmental Assessment? Or maybe it's simply a Categorical Exclusion. Either way, this is the section to select the pertinent NEPA level and list the file names and dates. Also send the documents for our review and inclusion in our administrative record. If the project warrants an EIS, we'll need the EIS named and its date, as well as the Record of Decision and its date. For an Environmental Assessment – we'll need the EA, its date, as well as the Finding of No Significant Impact and its date. For a CE, we just need the CE and its date. Whether it's an EIS, EA, or CE, we need electronic copies of and do review the environmental documents. Mark N/A for the parts that don't apply.

Also, note if the environmental documentation has been modified, re-evaluated or supplemented. If it has, please provide that reevaluation and list its date. We typically expect to see a reevaluation if the original NEPA document is 5 years or older.

Okay, moving right along we'll hit each of the other environmental topics to be addressed. Remember, we've made the application as comprehensive as possible, and we understand that not all sections apply to all cases.

Environmental Effects Abroad



2. **Environmental Effects Abroad** - Executive Order 12114 on the Environmental Effects Abroad of Major Federal Actions requires all federal agencies taking actions that may or will significantly harm the physical or natural environment of other nations or the global commons to take environmental considerations into account for that action.

a. Does the proposed project involve a bridge connection to Canada or Mexico?

☒ Yes ☐ No

1) If yes, cite location(s) (including page number(s) or section as appropriate) in the environmental documentation where environmental effects abroad are described *(If a proposed project has environmental impacts outside of the geographical borders of the United States and its territories it shall be discussed in the environmental document. Summarize the impacts, proposed mitigation, and the Department of State and Council on Environmental Quality consultation findings here,):*

☐ N/A, no international connection

Environmental effects abroad detailed in EIS Reevaluation, section 3.J, Summary of Transboundary Impacts, pages 123 and 124, Encl-12_EIS_Reeval_31Sep2024.
International Joint Commission Decision – received 6 December 2022, Encl-13_International_Joint_Commission_Decision_6Dec2022
Department of State confirmation no Presidential Permit required, dated 31 November 2023, Encl-14_DOS_Confirmation_31Nov2023.

Environmental Effects Abroad

For Environmental Effects Abroad, are we looking at an International Bridge that's going to connect the U.S. to either Canada or Mexico? Depending on your location that might apply or not. If not, mark no and move on. If yes, this is where you'll discuss environmental effects abroad in accordance with the provisions of Executive Order 12114, an EO that still stands. Any applicable documentation from the Department of State will need to be noted here and included as a support document.

Clean Water Act, Section 401, Water Quality Certification



3. **Clean Water Act, Section 401: Water Quality Certification** - Section 401 of the Clean Water Act of 1977(CWA) (33 U.S.C. 1251), as amended, prohibits Federal permitting or licensing agencies from issuing authorizations for construction activities having discharges into navigable waters, until the appropriate water quality certifying agency has issued a water quality certification or waiver procedures have been satisfied.


a. Has the certifying authority or authorities issued a Water Quality Certification (WQC), waiver or statement that the WQC is not required for the Coast Guard bridge permit?

- ☒ Issued for a Coast Guard bridge permit
- ☐ Issued for a U.S. Army Corps of Engineers (USACE) permit and the certifying authority confirmed WQC applies to the Coast Guard bridge permit
- ☐ Waived
- ☐ Denied
- ☐ Other/not required/still valid

NOTE: The USCG will not accept an application package as complete if a WQC, waiver, or statement from the appropriate regulatory body has not been obtained.

Next up is compliance with the Clean Water Act, a pertinent portion of which was updated at the end of 2023. First, we'll address Section 401 Water Quality Certification. In accordance with the 2023 update, please note that water quality certifications are required for each permit not simply the overarching project. Besides covering all Federal permits and approvals, also make especially certain that the Water Quality Certification covers the entire scope of the bridge project. In the spirit of efficiency, when you're applying for the project's water quality certification, include the Coast Guard in the WQC application and in any coordination activities such as the pre-filing meeting and determining the reasonable period of time. In the Coast Guard application, confirm if a WQC was issued, issued as part of a USACE permit, waived, dismissed, or something else. And be aware that if a proposed bridge connects two states, then two WQCs are required. While rare, we also understand there is a possibility that a WQC may not be needed such as in the case of a bridge clear spanning a waterway. Even in that case, confirmation that a WQC is not required needs to come from the certifying authority. A WQC is vital because the Coast Guard will not consider the application to be complete without a WQC or suitable justification that a WQC is not needed. And if the application is incomplete, then no permit is going to be issued.

<https://www.epa.gov/cwa-401/overview-cwa-section-401-certification>

<p>b. Name of Federal, State or Tribal certifying authority/authorities, date(s) of certification(s), and corresponding enclosure(s) title:</p> <p><input type="checkbox"/> N/A, WQC not issued</p> <p>Greatest State Department of Natural Resources, WQC dated 1 Jan 2024, Encl-15_GSDNR_WQC_1Jan2024. Adjacent State Department of Environmental Quality issued a new WQC on 14 Mar 2025, Encl-16_ASDEQ_WQC_14Mar2025.</p> <p>[Note: If a bridge connects two states, we need WQCs from both certifying authorities.]</p> <p>c. WQC expiration date (If not applicable, explain why):</p> <p><input type="checkbox"/> N/A</p> <p>WQC expires 1 Jan 2029.</p> <p>d. When a WQC is included in a USACE Nationwide Permit (NWP) or other state resource, general or regional permit, that WQC only applies to the USACE permit. The certifying authority/authorities must confirm that the WQC already issued through the NWP/resource/general/regional permit is valid for the CG permit. Include any confirmation correspondence and the date of the confirmation:</p> <p><input checked="" type="checkbox"/> N/A, WQC issued separately for the CG permit.</p> <p>-or-</p> <p>Greatest State Department of Environmental Conservation confirmed WQC applies to CG permit on 15 Mar 2025, Encl-17_GSDNR_email_15Mar2025.</p>	 <div data-bbox="997 793 1321 842">CWA, Section 401</div>
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Provide the name of the certifying authority, the WQC date plus the file name. If there is an expiration date, also insert that. Please forward the WQC as soon as it is received as we need to send it and the bridge permit application to the local EPA Region for a neighboring jurisdiction determination.

Let's talk a little about WQC's issued as part of an Army Corps of Engineers Nationwide, Regional or General Permit. Be advised that such embedded WQCs only cover the Army Corps permit. If you find yourself in this case, though, we advise asking the certifying authority if the WQC could also apply to the CG permit or if a separate WQC must be requested. It's possible that, because the project is the same, they'll allow the USACE WQC to cover the CG permit. Honestly, that would mean less work. However, only the state certifying authority can make that determination. And, if they do, then we need the date and clear confirmation from the certifying authority.

Speaking of certifying authorities, who is it typically for your projects?

CWA, Section 401



e. If waived, denied or not required, summarize why and cite enclosure(s) in the application package of supporting material:

☐ N/A, WQC issued separately for the CG permit (not waived, denied or not required).

WQC waived by Greatest State Environmental Protection Department (certifying authority) because bridge clear spans the waterway with no piers in the water, Encl-18_GSEPD_waiver_4May2024.

f. For permit modifications, include a new WQC for a Coast Guard bridge permit or a written confirmation from the certifying authority/authorities that the existing WQC has been reissued/renewed or is still valid for the proposed action.

☐ N/A, not a permit modification (WQC date(s) and file name above).

☐ New WQC attached (WQC date(s) and file name above):

☒ Written confirmation of WQC validity attached (cite enclosure):

Greatest State Department of Environment confirmed via email that the WQC issued on 4 Jul 2020 is still valid, Encl-19_GSDOE_WQC_email_21Jan2025.

Adjacent State Department of Environmental Protection confirmed via email that the WQC issued on 19 Jun 2021 is still valid, Encl-20_ASDEP_WQC_email_19Jun2025.

If the WQC is waived, denied, or not required, summarize why and include support documentation here.

For project or bridge changes that warrant a permit modification, note if a new WQC was issued or if the certifying authority confirmed the previously issued WQC is still valid for the modification. Per usual, fill in the file names, date, and deliver the documents. If a section does not apply, mark N/A.

Clean Water Act, Section 404, Discharge and Wetlands



4. **Clean Water Act, Section 404 and Protection of Wetlands** - Section 404 of the Clean Water Act (CWA) establishes a program to regulate the discharge of dredged or fill material into waters of the United States, including wetlands. Section 404 requires a permit from the US Army Corps before dredged or fill material may be discharged into waters of the United States. Also, per the Executive Order 11990 on the Protection of Wetlands, no federally approved project will occur in wetlands unless there is no practical alternative to constructing in the wetlands. As a result, the Coast Guard must analyze alternative locations which avoid taking wetlands. If no alternative locations or designs are practicable, then the Coast Guard must ensure that the project design includes all practicable measures to minimize wetland impacts.

a. Is the proposed project located in or adjacent to a wetland?

☒ Yes ☒ No

If no, provide explanation:

The bridge clear spans the waterway. It extends from bank to bank with no piers or other structures located in the river.

[Note: More often than not, bridges are located in or adjacent to a wetland. So this is one of the rare No examples.]

b. If yes, what is the area (acreage) of wetlands that will be permanently and temporarily impacted by the proposed project?

☐ N/A, minimal or no permanent or temporary impacts

Permanent impacts to 0.333 acres. Temporary impacts to 0.444 acres.

The other section of the Clean Water Act that usually applies is section 404 regarding the discharge of material into waters of the US. Related to that is wetlands impacts. Are we looking at a bridge project that's going to be either located in or positioned adjacent to a wetland? We're talking more than just marsh areas. When you're looking at this question, really ask yourself if there will be work in the water or in wet areas? If there is, then we'll need to know the acreage of wetlands that are going to be temporarily or permanently impacted.

c. Describe any wetland mitigation and cite enclosure(s) (including page number(s) or section, as appropriate).

☐ N/A, minimal or no permanent or temporary impacts

Wetland impact mitigation includes purchases from the Some Creek Mitigation Bank, as well as a portion of In-Lieu Fee mitigation. EIS dated Feb 2017, section 5.5

Wetlands, Encl-9_EIS_Jan2017. ROD, section 3 Mitigations, dated 30 Feb 2017, Encl-10_FHWA_ROD_30Feb2017. EIS Re-eval section 5.5, Encl-11_EIS_Reeval_31Sep2024, USACE 404 permit letter, Encl-8_USACE_404_permitletter_31Oct2023.

d. Does the proposed project include the discharge of dredged or fill material into waters of the United States, including wetlands (e.g., in-water work to construct or remove piers) that requires a USACE permit (nationwide authorization or individual)?

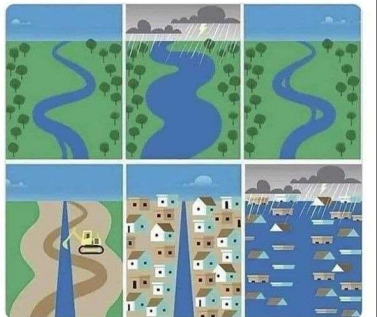
☒ Yes ☐ No

e. If yes, note the date the 404 permit was issued, the date it expires, and cite the enclosure title or provide a statement of no concern from the USACE:

USACE 404 Nationwide Permit (NWP) 14 letter issued on 19 June 2025. Encl-21_USACE_404_NWPletter_19Jun2025. NWP 14 issued 6 Dec 2022 and expires 6Dec 2027, Encl-22_USACE_NWP14_6Dec2022.

Someone: "hey, why are wetlands important?"

Wetlands:



CWA, Section 404

If there is wetland impact, please summarize the mitigation and name the source documents.

The next question is if there will be a discharge of fill or dredge material into waters of the US including, rivers, streams, and wetlands. Please list and provide the 404 permit and/or permit letter from the Corps. For nationwide permits, also provide the associated decision document or a link to it.

Which Army Corps Districts do you typically deal with?

<https://www.usace.army.mil/Missions/Locations/>

Coastal Zone Management Act

5. **Coastal Zone Management Act** - The Coastal Zone Management Act (CZMA) of 1972 (16 U.S.C. § 1451), as amended, and its implementing regulations (15 CFR Part 930), requires all projects located within the designated coastal zone of a state to be consistent with the State's federally approved Coastal Zone Management plan (CZMP).

a. Is the project located within the boundaries of a State's approved CZMP?

☒ Yes ☐ No, not located within the boundaries of an approved CZMP

b. If yes, has the State specifically excluded this activity from its federally approved CZMP?

☐ N/A, not located within the boundaries of an approved CZMP

☐ Yes ☒ No, activity is not excluded



c. List the certification and/or state concurrence or consistency, date(s), and corresponding file names. Cite page number or section in environmental document, if applicable:

☐ N/A, certification/concurrence not required

Coastal Zone Management concurrence issued with the WQC through the Greatest State Dept of Environmental Conservation Permit issued on 15 Mar 2025, Encl-23_GSDNR_WQC_CZM_15Mar2025.

-or-

CZMP certification dated 4 Aug 2024, Encl-24_CZMP-Certification_4Aug2024.

-or-

Coastal Management Consistency issued by State Department of Energy and Environment on 11 Sep 2023, Encl-25_Coastal_Consistency_11Sep2023.



Moving on to Coastal Zone Management, mark if the project is located within the boundaries of a state's approved Coastal Zone Management Plan. If yes, next note if the state excluded this activity or not. Then follow up with listing pertinent certification, state concurrence or consistency determinations. Fill in the file names, dates, and deliver the documents. If it does not apply, mark N/A.

What agency manages the coastal zone and conducts the Coastal Consistency Review for your projects?

<https://coast.noaa.gov/czm/consistency/>

Floodplain Management



6. **Floodplain Management** - Executive Order 11988 on Floodplain Management requires all federal agencies to avoid authorizing projects in the base (100-year) floodplain unless there is no practical alternative. By their very nature, most bridges are located within the base floodplain. Therefore, the Coast Guard must ensure that the project design includes all measures practicable to minimize floodplain impacts and to protect the natural and beneficial values of the floodplain.

a. Is the proposed project located in the base (100-year) floodplain?

☒ Yes ☐ No, not within the base (100-year) floodplain

b. If yes, is there an encroachment into the base (100-year) floodplain? (An encroachment does not exist when only the piers, pilings, or pile bents are located in the floodplain.)

☐ Yes ☒ No ☐ N/A, not in the base (100-year) floodplain

1) If yes, describe the encroachment, including any change to the floodplain elevation, quantities of fill removed (if any), fill placed, and net new fill (cite enclosure, page number, section, etc.):

☐ N/A, no encroachment

[Note: As an example, an encroachment might occur if bridge abutments require significant land buildup for support or if the proposed structure is a causeway.]

Floodplains

Flowing right along into Floodplain Management. FEMA's Flood Map Service Center and the Flood Insurance Rate Maps are useful resources. First, note if the bridge project is located in the base floodplain. The chance that the bridge is in a floodplain is pretty high considering bridges are over waterways that can flood. If that is the case, mark yes, and next confirm if there is or is not an encroachment. Then describe the encroachment plus any change to the floodplain elevation, any fill added or removed. Note an encroachment occurs when there is more than just piers, pilings, or pile bents in the floodplain.

<https://msc.fema.gov/portal/>

Floodplain Management

- 2) If no, describe how the project avoids encroachment into the base floodplain. Provide evidence through hydrologic and hydraulic analysis performed in accordance with standard engineering practice that the proposed project will not increase the base floodplain elevation at the project location, i.e., demonstrate no rise to the base floodplain elevation. Cite all enclosures (including page number(s) or section as appropriate):

☐ N/A, not in the base (100-year) floodplain

No encroachment because only the piers are located in the floodplain. EIS section 6.6 Floodplains, dated Feb 2017, Encl-9_EIS_Jan2017 and EIS Re-evaluation section 6.6 dated 31 Sep 2024, Encl-11_EIS_Reeval_31Sep2024.

[Note: It is rare for a bridge and the waterway it is over to NOT be in a floodplain or floodable area.]



FEMA Flood Map Service Center: Search By Address

Enter an address, place, or coordinates


Enter an address, place, or coordinates

Search

Whether you are in a high-risk zone or not, you may need flood insurance because most homeowners insurance doesn't cover flood damage. If you live in an area with low or moderate flood risk, you are 3 times more likely to experience flood than a fire in your home over the next 30 years. For many, a National Flood Insurance Program flood insurance policy could cost less than \$400 per year. Call your insurance agent today and protect what you've built. Learn more about <https://www.fema.gov/national-flood-insurance-program> to reduce flood risk damage.



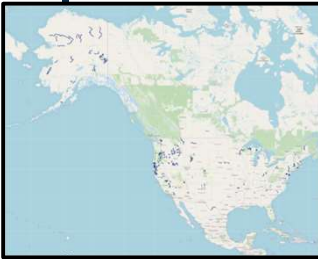
If no encroachment, note why. For example, only the piers will be in the water. The example here references the section in the EIS and EIS reevaluation where we can read up on floodplain impacts and mitigation.

	<h2 style="text-align: center;">Floodplain Management</h2>	<p>c. Is there a significant encroachment (constituting a considerable probability of loss of human life; likely future damage associated with the encroachment that could be substantial in cost or extent; or a notable adverse impact on natural and beneficial floodplain values) into the floodplain?</p> <p> <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> N/A, not in the base (100-year) floodplain </p> <p>1) If yes, provide documentation/coordination and cite enclosure(s) in the application package:</p> <p> <input type="checkbox"/> N/A, no significant encroachment </p> <p>[Note: A causeway might lead to a significant encroachment]</p> <p>d. Provide the 100-year flood elevation:</p> <p>88.8 feet above mean sea level.</p> <p>e. Provide low steel/member elevation for the proposed bridge:</p> <p>111.1 feet above mean sea level at sides of navigational channel.</p>

Next, mark if there is or is not a significant encroachment. That is one that contributes to a considerable probability of loss of life, substantial future damage, or notable adverse impact.

After that, we ask for the 100-year flood elevation and the low steel or low member elevation for the proposed bridge. These are pieces of data that we include in our public notice.

Wild and Scenic Rivers



7. **Wild and Scenic Rivers** - Section 7 of the Wild and Scenic Rivers Act of 1968 (16 U.S.C. § 1271), as amended, prohibits the issuance of any federal permit for construction of projects having adverse impacts on a river, or a proposed river, and adjacent lands with values qualifying it for protection under this Act.

a. Is the river involved in the proposed bridge project a designated or proposed Wild and Scenic River segment or listed on the Nationwide Rivers Inventory?

☐ Yes ☒ No, not a designated or proposed waterway

b. If yes, list impacts and mitigation, summarize correspondence with the river-administering agency and cite location(s) (including page number(s) or section as appropriate) in the application package:

☒ N/A, not a designated river

[Note: It is rare, but not impossible, that a bridge over a scenic river requires a CG permit. Please check the National Wild and Scenic River System inventory and then check with the CG District Bridge Office.]

For Wild and Scenic Rivers, we advise checking the Wild and Scenic Rivers Inventory. Then mark the appropriate box to either confirm no Wild or Scenic River impacts or yes. If yes, list impacts, mitigations, and support document file names. This is usually not the case, but it's wise to verify.

www.rivers.gov/map.php

Coastal Barrier Resources Act

8. **Coastal Barrier Resources Act** - The Coastal Barrier Resources Act (CBRA) established the Coastal Barrier Resources System and prohibits federal funding for building and development in undeveloped portions of designated coastal barriers, including the Great Lakes unless the project falls under an exception to the CBRA.

a. Does the proposed project connect to a unit of the Coastal Barrier Resources System (CBRS)?

☒ Yes ☐ No, there is no connection to any unit of the CBRS

b. If yes, summarize the CBRA-related impacts, proposed mitigation, and any U.S. Fish and Wildlife Service (USFWS) regional office consultation findings. Cite corresponding enclosure(s). Cite page number or section in environmental document, if applicable:

☐ N/A, no connection to CBRS

The Cape Coolness National Seashore and Awesome Island National Wildlife Refuge are classified as Coastal Barrier Resources. However, Cape Coolness National Seashore is a unit of the National Park System and Awesome Island National Wildlife Refuge is a unit of the National Wildlife Refuge System. Within the Coastal Barrier Resource System both are classified as "otherwise protected areas" (OPAs) due to their status as NPS and FWS units. As a result of being classified as OPAs, the only Federal spending prohibition within such areas is the prohibition on Federal flood insurance. Because of this status and the activities involved, no further compliance with the CBRA is required. EIS section 8.8 dated Jan17, Encl-9_EIS_Jan2017, and EIS Re-evaluation dated 31 Sep 2024, Section 8.8 Encl-11_EIS_Reeval_31Sep2024.



c. If yes, and the project is federally funded, cite enclosure of Section 6 exception in the application package and any correspondence with the USFWS:

☒ N/A, no connection to CBRS

[Note: It is uncommon for a bridge to be located in a Coastal Barrier Unit; however, be sure to check the Coastal Barrier Resource System Mapper to be sure.]



For the Coastal Barrier Resources System, we ask applicants to check the Fish and Wildlife Coastal Barrier Resources System Mapper for coastal barrier impacts. To find the Fish and Wildlife Services mapping tools, use the search term Coastal Barrier Resources mapper. If your proposed project is going to connect to a unit of a coastal barrier resource system, mark yes. Then summarize the impacts, proposed mitigations, consultation findings and name the support documents. Also provide the section 6 exception as part of the application package plus any pertinent Fish and Wildlife Service correspondence. For projects outside the CBRS units or otherwise protected areas, mark No and the N/A boxes.

www.fws.gov/program/coastal-barrier-resources-act/maps-and-data

Land and Water Conservation Fund Act, Section 6(f)



9. **Land and Water Conservation Fund Act** - Section 6(f) of the Land and Water Conservation Fund Act (LWCFA) assures that once an area has been funded with LWCFA assistance, it is continually maintained in public recreation use unless the National Park Service (NPS) approves substitution property of reasonably equivalent usefulness and location and of at least equal fair market value. The Secretary must approve all conversions of property acquired or developed with LWCFA assistance under this section to other than public outdoor recreation uses.

a. Does the proposed project involve a conversion of land or facilities funded under Section 6(f) of the LWCFA?

☒ Yes ☐ No, there are no impacts to any LWCFA properties

b. If yes, summarize and include correspondence with the NPS and authorization from the Secretary of the Interior for that conversion and cite enclosure title(s) (including page number(s) or section as appropriate):

☐ N/A, no LWCFA properties impacted

The Really Fun Park located on the south side of the waterway will temporarily lose one quarter acre of campsites during construction. The loss was mitigated through improvements to other parts of the park. Environmental Assessment, section 3.16, Section 4(f) and 6(f) Properties details the consultation, coordination and mitigation agreements. EA section 3.2 dated 25 May 2023, Encl-26-EA_25May2023

If a proposed project involves a conversion of land or facilities funded by Section 6(f) of the Land Water Conservation Fund Act (LWCFA), select yes, summarize, and provide the correspondence and authorization from the National Park Service under the Department of Interior. While this is not common, we do sometimes see projects impacting adjacent boat ramps or other recreation areas that were established through the LWCF. If this is not the case mark no and N/A and move on.

<https://lwcf.tplgis.org/mappast/>

National Marine Sanctuaries and Marine Protected Areas



10. **National Marine Sanctuaries Act and Marine Protected Areas** - Section 304(d) of the National Marine Sanctuaries Act (16 U.S.C. § 1434(d)) requires interagency consultation between NOAA and federal agencies taking actions, including authorization of private activities, "likely to destroy, cause the loss of, or injure a sanctuary resource."

a. Is the proposed project in or adjacent to a National Marine Sanctuary?

☐ Yes ☒ No, not located in or adjacent to a National Marine Sanctuary

b. If yes, list National Marine Sanctuaries Act coordination, date(s), and enclosure(s) and NOAA comment, if applicable:

☒ N/A, not in or adjacent to a National Marine Sanctuary

[Note: It is rare, but possible, to have a bridge in a National Marine Sanctuary, however check NOAA's National Marine Sanctuaries website to make sure.]

c. Is the proposed bridge(s) likely to destroy, cause loss of, or injure a resource of a National Marine Sanctuary?

☐ Yes ☒ No

d. If yes, summarize and include evidence of consultation with NOAA's Office of National Marine Sanctuaries and the agency's findings/conditions and cite location(s) (including page number(s) or section as appropriate) in the application package. If no, provide explanation:

☒ N/A, proposed bridge is not likely to destroy, cause loss of, or injure a resource of a National Marine Sanctuary

[Click here to enter text.](#)

Is the project in a National Marine Sanctuary? It's unlikely but still check NOAA's national marine sanctuary website before marking the applicable No and N/As and moving on. If your bridge project does occur in or adjacent to a sanctuary, list and provide the coordination with NOAA regarding compliance. Also note if the proposed bridge is likely to destroy, cause loss of or injure a national marine sanctuary resource. Then summarize and provide the coordination documents.

Marine Protected Areas

Executive Order 13158 on Marine Protected Areas requires all federal agencies whose actions affect the natural or cultural resources that are protected by a Marine Protected Area (MPA) to identify such actions and, to the extent permitted by law and to the maximum extent practicable, avoid harm to the natural and cultural resources that are protected by an MPA.

a. Is the proposed project in or adjacent to an MPA?

☒ Yes ☐ No, not located in or adjacent to an MPA

b. If yes, will the proposed project affect the natural or cultural resources that are protected by the MPA?

☐ N/A, project is not in or adjacent to an MPA

☐ Yes ☒ No

If no, provide evidence:

Per EA section 4.4 dated 25 May 2023, the project location is adjacent to a Marine Protected Area. Best management practices, including project area demarcation and worker training, will be incorporated to prevent disturbance of the protected section. Encl-26-EA_25May2023.





c. If yes, include evidence of correspondence with the MPA Center, if applicable, and cite location(s) (including page number(s) or section as appropriate) in the application package:

☒ N/A, proposed project does not affect natural or cultural resources that are protected by the MPA

EA appendix N, MPA letter from NOAA dated 31 Nov 2023. Encl-26-EA_25May2023.

The Executive Order on Marine Protected Areas still requires that efforts be undertaken to avoid to the extent possible harm to natural and cultural resources protected by the MPA. Use NOAA's Marine Protected Area Interactive Map to see if your bridge project is to be constructed in or around a Marine Protected Area. If a project is in or adjacent to an MPA, mark yes, then answer if the project will affect the protected natural or cultural resources. If not near an MPA or no effects, mark No or N/A, as appropriate.

Endangered Species Act

11. Endangered Species Act and Fish and Wildlife Coordination Act - Section 7 of the Endangered Species Act of 1973 (ESA) (16 U.S.C. § 1531), as amended, requires each Federal agency to ensure that any action authorized, funded, or carried out by the agency is not likely to jeopardize the continued existence of any endangered or threatened species or result in the destruction or adverse modification of critical habitat.

a. Are there federally designated threatened or endangered species and/or critical habitat in the area that the proposed project is located?

☒ Yes ☐ No there are no federally designated threatened or endangered species and/or critical habitat in the area of the proposed project

If no, provide explanation:


[Note: It is uncommon to NOT have threatened/endangered species or habitat in the project area.]

b. May the proposed project affect federally designated threatened or endangered species and/or critical habitat?

☒ Yes ☐ No, there are no potential impacts to protected species and/or critical habitat, nor is any consultation required

If no, provide explanation:

[Click here to enter text.](#)



More often than not, bridge projects are located either within a critical habitat or may pose impacts to a species protected under the auspices of the Endangered Species Act. Like previous environmental topics, as well as those to come, Coast Guard will seek to ensure compliance with this legislation. The first two questions to answer include: are there federally designated threatened or endangered species or critical habitat in the proposed project's area? Second, may the proposed project affect such designated species or critical habitat? Mark the appropriate boxes and, if no, explain why there are no impacted species or habitats. Be sure to check with both services: National Marine Fisheries Service (NMFS) and the US Fish and Wildlife Service (FWS).

<https://ipac.ecosphere.fws.gov> [IPaC Home](#)

<https://www.fisheries.noaa.gov/topic/consultations/endangered-species-act-consultations>

c. If yes, was there formal or informal consultation with the United States Fish and Wildlife Service (USFWS) or the National Marine Fisheries Service (NMFS)?

☐ N/A

☒ Formal consultation

☒ Informal consultation

d. If formal, provide date(s) and attach biological assessment/evaluation, biological opinion, and any other relevant correspondence and cite location(s) (including page number(s) or section as appropriate) in application package:

☐ N/A, formal consultation not required

Formal consultation completed with Biological Opinions from both USFWS and NMFS, dated 20Mar2023 and 21Sep2023, respectively. The associated Biological Assessments were completed on 20Sep2022 and 21Mar2023. Encl-27_BioOpinion_USFWS_20Mar2023, Encl-28_BioOpinion_NMFS_21 Sep2023, Encl_27a_BA_IPAC_USFWS_20Sep2022, Encl_28a_BA_NMFS_21Mar2023.

e. If informal, provide dates and include correspondence or documented phone conversations with and from USFWS/NMFS and cite location(s) (including page number(s) or section as appropriate) in the application package:

☐ N/A, informal consultation not required

IPAC completed 17Mar2024, Encl-29_IPAC_17Mar2024. Informal consultation completed via email with NMFS on 21Jun2023, Encl-29a_NMFSEmail_21Jun2023.

Endangered Species Act

While NMFS's process is a little more old school, the USFWS has a handy Information for Planning and Consultation (IPaC) tool and a Consultation Package Builder that streamlines the FWS environmental review process. Does anyone have experience with the IPAC? Give me a thumbs up or other reaction.

If answers to the previous questions were yes, please indicate if there was formal, informal or both types of consultation and which service that was with. Describe the consultations and the file names of the provided documents such as email confirmations, biological assessments, biological opinions, and/or references in the environmental documents. For this example, there was both formal and informal consultation.

Fish and Wildlife Coordination Act



The Fish and Wildlife Coordination Act (FWCA) (16 USC § 742, et seq.) provides the basic authority for the USFWS' involvement in evaluating impacts to fish and wildlife from proposed water resource development projects. It requires that fish and wildlife resources receive equal consideration to other project features. It also requires Federal

agencies that construct, license, or permit water resource development projects to first consult with the USFWS (and NMFS in some instances) and the State fish and wildlife agency regarding the impacts on fish and wildlife resources and measures to mitigate these impacts.

- a. Describe any correspondence with and recommendations from USFWS, NMFS, and the relevant state wildlife agency regarding FWCA coordination and cite location(s), date(s) and enclosure(s) (including page number(s) or section as appropriate) in the application package:

☐ None

FWCA coordination addressed during informal consultation with USFWS, Encl-27-BioOpinion_USFWS_20Mar2023, IPAC completed 17Mar2024, Encl-29_IPAC_17Mar2024.

The Fish and Wildlife Coordination Act pertains to federal agencies that construct, license, or permit water resource development projects. Usually, the FWCA consultation requirements are managed concurrently with the endangered species. If there was FWCA coordination, please describe and note the file names here.

Essential Fish Habitat



12. **Magnuson-Stevens Fishery Conservation and Management Act** - The Magnuson-Stevens Fishery Conservation and Management Act (Magnuson-Stevens Act) (16 U.S.C. § 1855), as amended, requires Federal agencies which fund, permit, or carry out activities that may adversely impact Essential Fish Habitats (EFH) to consult with the National Marine Fisheries Service (NMFS) regarding potential adverse effects of actions on EFH.

- a. Will the proposed project likely adversely affect designated EFH as defined in the Magnuson-Stevens Act?

☐ Yes ☒ No, there are no potential adverse impacts to designated EFH

If no, provide explanation:

Per NOAA's EFH Mapper, there are no EFHs in the project area.

- b. If yes, identify location of EFH assessment and list relevant correspondence with NMFS in the application package.

☐ N/A, no adverse effects on designated EFH

Per consultation with NMFS, EFH impacts are minimal and will be mitigated as noted in the NMFS Biological Opinion, Encl-28_BioOpinion_NMFS_21 Sep2023.

For the Magnuson Stevens Fishery Conservation and Management Act, check NOAA's Essential Fish Habitat mapper. Then, if your project looks like it will impact essential fish habitat, engage and consult with the National Marine Fisheries Service. Similar to the FWCA above, when this Act applies, it is usually covered concurrently with the Endangered Species Act coordination. Mark the appropriate boxes and provide the reference documents or, if no effects, mark no and N/A and move on.

www.habitat.noaa.gov/apps/efhmapper/

Marine Mammal Protection Act

13. **Marine Mammal Protection Act** - The Marine Mammal Protection Act (MMPA) (16 USC § 1361, et seq.) prohibits, with certain exceptions, the take of marine mammals in U.S. waters and by U.S. citizens on the high seas, and the importation of marine mammals and marine mammal products into the U.S. If a take may occur, an Incidental Take Authorization may be necessary. The National Marine Fisheries Service (NMFS) covers seals, sea lions, whales, dolphins and porpoises. Meanwhile, the U.S. Fish and Wildlife Service (USFWS) manages polar bears, the Pacific Walrus, Northern Sea Otters and the West Indian Manatee.

a. Describe any possibility of impacts to marine mammals. List MMPA coordination, date(s), enclosure(s), and NMFS or USFWS comment, if applicable. If coordination is not required, explain why:

Per consultation with NMFS, marine mammal impacts are minimal and will be mitigated as noted in the NMFS Biological Opinion, Encl-28_BioOpinion_NMFS_21 Sep2023.

b. Does the proposed project involve a “take” of marine mammals as defined in the MMPA?

☐ Yes ☒ No, there are no “takes” of mammals protected by the MMPA

1) If yes, include the incidental harassment authorization or letter of authorization from NMFS, any relevant correspondence and summarize the impacts, proposed mitigation and NMFS regional office consultation findings:

☒ N/A, no “take” of marine mammals protected by the MMPA

[Click here to enter text.](#)



2) If no, explain why the project does not involve a “take” of marine mammals, including description of any preventative mitigations:

☐ N/A, project involves a take as described above

No take is anticipated due to incorporation of best management practices such as incorporation of slow starts, daily marine mammals surveys, turbidity and bubble curtains. Per consultation with NMFS, marine mammal impacts are minimal and will be mitigated as noted in the NMFS Biological Opinion, Encl-28_BioOpinion_NMFS_21 Sep2023.

The Marine Mammal Protection Act ensures protection for a wide variety of mammals. National Marine Fisheries Service, NMFS, covers seals, sea lions, whales, dolphins and porpoises. Meanwhile, US Fish and Wildlife Service manages polar bears, the Pacific Walrus, Northern Sea Otters and the West Indian Manatee. We ask that you note if there is possibility that the proposed project might impact marine mammals and share the related coordination. Like the previous ones, it’s possible that this coordination is also included in the Endangered Species Act consultation. Please note if the project will involve a “take” of marine mammals and, if so, name and provide the related documentation such as an Incidental Harassment Authorization or a Letter of Authorization. If no takes, mark accordingly.

Migratory Bird Treaty Act

14. **Migratory Bird Treaty Act** - Migratory Bird Treaty Act (MBTA) (16 U.S.C. § 703-712) made it illegal to take any migratory bird, nest, egg, or part or any bird protected under the Act except under the terms of a valid permit issued by the USFWS.

a. Describe any possibility of impacts to migratory birds. List MBTA coordination, date(s), and enclosure(s):

Some possible impacts to migratory birds were identified in the Biological Assessment and Biological Opinion. Consultation completed with USFWS, dated 20Mar2023. The associated Biological Assessment was completed on 20Sep2022. Encl-27-BioOpinion_USFWS_20Mar2023, Encl-27a_BA_IPAC_USFWS_20Sep2022.

b. Does the proposed project involve a potential take of migratory birds as defined in the MBTA?

☐ Yes ☒ No, there are no potential "takes" of birds protected by the MBTA

1) If no, provide explanation and describe any preventative mitigations:

☐ N/A, project involves a take as described above

Mitigation methods as described in the Biological Opinion will be incorporated during construction activities, including conducting surveys and putting up anti-bird barriers during non-nesting seasons.

2) If yes, is a permit required?

☐ Yes ☐ No ☒ N/A, no potential take

When it comes to anticipated impacts to migratory bird species, you'll summarize that in this section. Birds are fairly ubiquitous, so don't fly by this one too fast. We recommend reviewing USFWS's handy Migratory Bird Permitting Handbook. You can also use the Rapid Avian Information Locator to identify possible species in your project area. Please describe possible impacts to our feathered friends and note yes or no if your project will or will not involve a potential take of a migratory bird species. If no, summarize why and/or what mitigations will be employed. If yes...

https://www.fws.gov/sites/default/files/policy/files/migratory_bird_permitting_handbook_2024.pdf

Rapid Avian Information Locator (RAIL)

3) If a permit is required, include it and any correspondence with USFWS and cite location(s) (including page number(s) or section as appropriate) in the application package:

☒ N/A, no permit required

[Click here to enter text.](#)



c. Have the project's effects on migratory birds been evaluated in accordance with the executive order for the Responsibility of Federal Agencies to Protect Migratory Birds?

☐ Yes ☐ No ☒ N/A, no effects on migratory birds

d. If yes, describe coordination with USFWS conducted in accordance with the executive order. Provide date(s), enclosure(s), and USFWS comment, if applicable. If no, explain why:

☒ N/A, no coordination required

[Click here to enter text.](#)

Migratory Bird Treaty Act

Send us the permit and note the filename here or reference the environmental document.

For the question about evaluating effects in accordance with Executive Order 13186, Responsibilities of Federal Agencies to Protect Migratory Birds, mark either Yes or N/A. The EO applies to Federal agencies and not individual projects. If yes, then note that Federal agencies are working within their MOUs with the USFWS.

Bald and Golden Eagle Protection Act

15. **Bald and Golden Eagle Protection Act** - The two species of eagles that are native to the United States have additional protection under the Bald and Golden Eagle Protection Act (BGEPA)(16 USC § 668-668c). USFWS may issue permits for the take, possession, or transport of bald or golden eagles, as well as their parts, nests, and eggs.

a. Describe any impacts to eagles, nests, parts, or eggs. List BGEPA coordination, date(s), and enclosure(s):

☒ N/A, there are no impacts to Bald or Golden Eagles including nests, parts, or eggs protected by the BGEPA

A Bald Eagle survey was conducted on 4Jul2024 and no nests were located in the vicinity of the project. Encl-30_EagleSurvey_4Jul2024

b. May the proposed project take or disturb bald or golden eagles (including active and inactive nests) as defined in the BGEPA?

☐ Yes ☒ No

1) If no, provide explanation and describe any preventative mitigations:

A Bald Eagle survey was conducted on 4Jul2024 and no nests were located in the vicinity of the project. Encl-30_EagleSurvey_4Jul2024

2) If yes, is a permit required?

☒ N/A, no potential take or disruption

☐ Yes ☐ No

3) If a permit is required, summarize the proposed mitigation and USFWS Regional Office consultation findings and cite location(s) (including page number(s) or section as appropriate) in the application package:

☒ N/A, no permit required

[Click here to enter text.](#)

The Bald and Golden Eagle Protection Act may or may not apply depending on where your bridge project is located and who is nesting nearby. The USFWS has a website dedicated to Eagle Management and we recommend checking it out. Describe for us if there are going to be any impacts to either Golden or, more likely, Bald Eagles. Also note if there will or will not be a take or disturbance. If no, summarize the explanation and/or preventive mitigation. If yes, summarize the proposed mitigation and consultation and share any permits. Generally, most of this information will be noted in the environmental document or next survey. List the filename and let us know where we can find that information by page or section number.

<https://www.fws.gov/program/eagle-management>

<h2 style="text-align: center;">Invasive Species</h2>	<div data-bbox="220 562 550 892"> </div> <div data-bbox="597 394 1411 932"> <p>16. <u>Invasive Species</u> - Executive Order 13112 on Invasive Species requires all federal agencies whose actions may affect the status of invasive species to prevent the introduction of invasive species and not authorize, fund, or carry out actions that it believes are likely to cause or promote the introduction or spread of invasive species.</p> <p>a. Does the proposed project have potential to introduce or foster the spread of invasive species?</p> <p><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No, there is no potential introduction or spread of invasive species</p> <p>b. If yes, cite the document that describes measures that will be taken to minimize this risk and location(s) (including page number(s) or section as appropriate) in the application package:</p> <p><input type="checkbox"/> N/A, no potential impacts from invasive species</p> <p>EIS Section 6.6 and Reevaluation section 6.6 describes the best practices that will be incorporated to prevent the spread of invasive species, Encl-9_EIS_Jan2017 and Encl-11_EIS_Reeval_31Sep2024.</p> </div>

Invasive species are a big deal. Does your proposed bridge project have the potential to introduce an invasive species into the local ecosystem or are you implementing best practices to prevent that? If so, this is the spot where you'll mark yes or no and discuss those impacts or prescribed mitigation efforts. Like so many other topics, this may be detailed in the environmental document. For example, the document may site something like "All off-road equipment and vehicles shall be cleaned prior to entering the construction site to remove all soil, seeds, vegetation, or other debris that could contain seeds or reproductive portions." Cite and send any support documents. What are some of the invasive species that you've had to deal with and typical mitigation methods?

Historical and Cultural Resources

NHPA, Section 106



17. **Historical and Cultural Resources** - Federal agencies are required to take into account the effects of their undertaking on sites, structures, etc., protected by the historic and cultural resource laws and regulations identified in this section.

- a. In accordance with Section 106 of the National Historic Preservation Act of 1966 (54 U.S.C. § 306108 et seq.), as amended, does the proposed project have potential to impact properties (including submerged abandoned shipwrecks) listed in or eligible for inclusion in the National Register of Historic Places (NRHP)?

☒ Yes ☐ No

Identify any State Historic Preservation Officer(s) (SHPO) and/or Tribal Historic Preservation Officer(s) (THPO) with oversight of the project

Greatest State Historic Preservation Officer, Adjacent State Historic Officer,
Important Nation THPO, Another Important Nation THPO

Let's now dig into the historical and cultural resources topic. Since bridges are built to last several decades or more and if you are replacing or modifying an existing bridge, then you'll likely have to deal with Section 106 of the National Historic Preservation Act. So first, does the proposed project have the potential to impact properties listed or eligible for inclusion in the National Register of Historic Places? Mark yes or no. Then identify the State Historic Preservation Officers (SHPOs) and, possibly, Tribal Historic Preservation Officers (THPO) with oversight of the project.

<p>b. If yes, identify the documents that describe the effects and appropriate mitigation and provide evidence of consultation with the SHPO and/or THPO, and the Advisory Council on Historic Preservation, if applicable, and cite location (s) (including enclosure names, and page number(s) or section as appropriate) in the application package. Please check all documents that are included and cite any corresponding enclosures:</p> <p><input type="checkbox"/> N/A, no potential impacts to properties listed in or eligible for inclusion in the NRHP</p> <p><input checked="" type="checkbox"/> Historic properties affected but no adverse effect determination, cite enclosure(s)</p> <p>Adverse effect determinations by Greatest State Historic Preservation Officer and Adjacent State Historic Officer dated 11 Nov 2023 and 6 Dec 2024, Encl-31_SHPO_Letters_11Nov_6Dec2024.</p> <p><input checked="" type="checkbox"/> Historic properties adversely effected and Memorandum of Agreement or Programmatic Agreement completed, cite enclosure(s)</p> <p>The existing 90 year old bridge is an historic property and will be adversely impacted through the replacement project. A Memorandum of Agreement dated 28 Nov 2024 was established between Greatest State Historic Preservation Officer, Adjacent State Historic Officer, Important Nation THPO, Another Important Nation THPO, and Greatest State Department of Transportation. Encl_32_Sec106MOA_28Nov2024.</p> <p><input type="checkbox"/> No historic properties effected determination, cite enclosure(s)</p> <p>Click here to enter text.</p>	<p>c. For projects involving federal lands also provide:</p> <p><input type="checkbox"/> Archeological clearances, cite enclosure(s)</p> <p>Click here to enter text.</p> <p><input type="checkbox"/> Archeological reports, cite enclosure(s)</p> <p>Click here to enter text.</p> <p>d. Provide any other information regarding Section 106 process, such as public meetings or unique information, and corresponding enclosure(s) (including page number(s) or section as appropriate):</p> <p><input checked="" type="checkbox"/> None</p> <p>Click here to enter text.</p> <p>e. Does the proposed project have potential to involve Native American cultural items as identified by the Native American Graves Protection and Repatriation Act?</p> <p><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>1) If yes, please identify Tribal Nations that might be impacted, summarize impacts, identify any proposed mitigation, and summarize any consultation findings. Cite any corresponding enclosure(s) and National Park Service (NPS) comment, if applicable. Cite page number or section in environmental document, if applicable:</p> <p><input checked="" type="checkbox"/> N/A, no potential involvement of resources as identified by the Native American Graves Protection and Repatriation Act</p> <p>Click here to enter text.</p>
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Section 106

If there are potential impacts, please identify and provide the related documents which may take the form of a no adverse effect determination letter, a memorandum of agreement, or a programmatic agreement. For projects including federal lands, we also ask for archeological clearances and reports. In addition to that, please summarize other Section 106 information or activities such as public meetings or other activities. Related but separate, note if the project has the potential to involve Native American cultural items per the Native American Graves Protection and Repatriation Act.

<p>f. Does the proposed project involve or have the potential to involve any Native American historic resources identified by the American Indian Religious Freedom Act of 1978?</p> <p><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>1) If yes, please identify Tribal Nations that might be impacted, summarize impacts, identify any proposed mitigation, and summarize consultation findings. Cite any corresponding enclosure(s) and appropriate tribal consultation findings, if applicable. Cite page number or section in environmental document, if applicable:</p> <p><input checked="" type="checkbox"/> N/A, no potential involvement of resources as identified by the American Indian Religious Freedom Act</p> <p>Click here to enter text.</p> <p>g. Does the proposed project involve or have the potential to involve a historic or prehistoric ruin or monument as identified by the Antiquities Act of 1906?</p> <p><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>1) If yes, please summarize impacts, any proposed mitigation, and consultation findings. Cite any corresponding enclosure(s) and appropriate SHPO, NPS or other appropriate agency consultation findings, if applicable. Cite page number or section in environmental document, if applicable:</p> <p><input checked="" type="checkbox"/> N/A, no potential impacts of resources as identified by the Antiquities Act</p> <p>Click here to enter text.</p>	<p>h. Does the proposed project involve or have the potential to involve an archaeological resource or site identified by the Archeological Resources Protection Act of 1979?</p> <p><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>1) If yes, please summarize impacts, any proposed mitigation, and consultation findings. Cite corresponding enclosure(s) and appropriate SHPO, NPS or other appropriate agency consultation findings, if applicable. Cite page number or section in environmental document, if applicable:</p> <p><input checked="" type="checkbox"/> N/A, no potential involvement of resources as identified by the Archeological Resources Protection Act</p> <p>Click here to enter text.</p> <p>i. Does the proposed project involve or have the potential to involve a shipwreck as identified by the Abandoned Shipwreck Act?</p> <p><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>1) If yes, please summarize impacts, any proposed mitigation, and consultation findings. Cite any corresponding enclosure(s) and appropriate SHPO, NPS or other appropriate agency consultation findings, if applicable. Cite page number or section in environmental document, if applicable:</p> <p><input type="checkbox"/> N/A, no potential involvement of resources as identified by the Abandoned Shipwreck Act</p> <p>A shipwreck is located within 50 yards of pier 3. Construction will be coordinated to avoid disturbing the shipwreck. Barges will only be moored on the opposite side of the bridge. Encl-33_SHPO_email_28Jan2023.</p>
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Other Acts

There are a few more historic and cultural resource acts to consider. Please note if the project involves or has the potential to involve historic resources identified by the American Indian Religious Freedom Act, the Antiquities Act, the Archeological Resources Protection Act, and the Abandoned Shipwreck Act. If there are impacts or potential impacts, list the document names and provide the files. If not, mark no and N/A.

Clean Air Act



18. Clean Air Act - Section 176(c) of the Clean Air Act (CAA)(42 U.S.C. § 7401, as amended), prevents the Coast Guard from approving any project or from issuing any permit for actions not conforming to the provisions of an approved Federal Implementation Plan (FIP) or to a State Implementation Plan (SIP).

a. Is the project in an area of maintenance or nonattainment for each of the criteria pollutants in the National Ambient Air Quality Standards (NAAQS)?

☒ Yes ☐ No

b. If project occurs in a nonattainment or maintenance area, do the transportation or general conformity regulations, or both, apply?

☐ N/A, project does not occur in a nonattainment or maintenance area

☐ General ☒ Transportation

c. Is the project exempt from a transportation conformity analysis for any of the reasons listed in 40 CFR § 93.126?

☐ Yes ☒ No

1) If yes, identify the reason(s):


☒ N/A, project is not exempt from a transportation conformity analysis

[Click here to enter text.](#)

Clean Air is important to us all and here we ask about compliance with the Clean Air Act. Is the project in an area of nonattainment or maintenance? Note so. If the project is in a nonattainment or maintenance area, do transportation or general conformity regulations apply? Enter that answer. Is the project exempt from a transportation conformity? Note and justify that.

<p>d. Is the project exempt from a general conformity analysis for any of the reasons listed in 40 CFR § 93.153(c)?</p> <p><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>1) If yes, identify the reason(s):</p> <p><input checked="" type="checkbox"/> N/A, project is not exempt from a general conformity analysis</p> <p>Click here to enter text.</p> <p>e. If general conformity applies, is the project listed in a conforming State Implementation Plan?</p> <p><input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> N/A, general conformity does not apply</p> <p>f. If a general conformity determination was prepared, include the draft and final determinations and any relevant correspondence and cite their title (including page number(s) or section as appropriate) in the application package:</p> <p><input checked="" type="checkbox"/> N/A, a general conformity determination was not prepared</p> <p>Click here to enter text.</p>	<p>g. If transportation conformity applies, is the project listed in a conforming State Implementation Plan (SIP), Transportation Improvement Program (TIP), Regional Transportation Plan (RTP), or Federal Implementation Plan (FIP)?</p> <p><input type="checkbox"/> N/A, transportation conformity does not apply</p> <p><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>1) If yes, identify the plan and cite location of information regarding listing in the application package (including page number(s) or section as appropriate):</p> <p><input type="checkbox"/> N/A, transportation conformity does not apply so project is not listed in any SIP, TIP, RTP or FIP</p> <p>Greatest State Transportation Improvement Project dated 20 Apr 2022, Encl-34_GSTIP_20Apr2022.</p> <p>h. If transportation conformity applies, does the project contribute to any new localized CO, PM10, or PM2.5 violations or increase the frequency or severity or any existing violations of the same?</p> <p><input type="checkbox"/> N/A, transportation conformity does not apply</p> <p><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>1) If yes, cite enclosure title and (including page number(s) or section as appropriate):</p> <p><input checked="" type="checkbox"/> N/A, transportation conformity does not apply and project does not contribute to any violations</p> <p>Click here to enter text.</p>
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Clean Air Act



What about the project being exempt from a general conformity analysis? If a general conformity applies, is the project listed in the State Implementation Plan (SIP)? If so, please list and provide the SIP as a support document. If a general conformity was prepared, note so and provide a copy of the determination. If a transportation conformity applies, is the project listed in a State Implementation Plan, Transportation Improvement Program, Regional Transportation Plan or Federal Implementation Plan? Note so and provide the document. When a transportation conformity applies, does the project contribute new or increase carbon monoxide or particulate matter violations? Wrap up the Clean Air section with that answer.

Hazardous Materials, Substances, and Wastes



19. Hazardous Materials, Substances and Wastes

- a. Does the proposed project involve or is it located near a Superfund site or impact, or have the potential to impact any site regulated under the Comprehensive Environmental Response, Compensation and Liability Act (CERCLA), Resource Conservation and Recovery Act (RCRA) or State law regulating hazardous materials, substances or wastes?

☒ Yes ☐ No

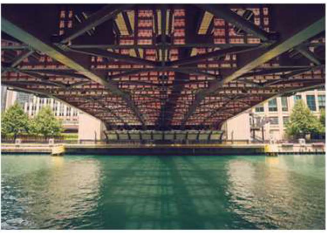

- b. If yes, describe the involvement and cite the location(s) (including page number(s) or section as appropriate) in the NEPA or other document where hazardous materials, substances or wastes are discussed:

The proposed abutment is located adjacent to a property contaminated by an abandoned chemical plant. The location has been remediated; however, monitoring especially of groundwater continues. Section 4.22, EIS dated Feb 2017, Encl-9_EIS_Jan2017, and section 4.2 of the EIS Reevaluation dated 31 Sep 2024, Encl-11_EIS_Reeval_31Sep2024.




- c. If no, provide any additional pertinent information and cite any analysis conducted:

[Click here to enter text.](#)

With hazardous materials, substances, and wastes, the end is in sight... well, not the end of ends, but we're nearly at the end of this walk through. We ask you to note if the proposed bridge project is located near a Superfund site or any sites regulated under the Comprehensive Environmental Response, Compensation and Liability Act or the Resource Conservation and Recovery Act. If so, describe the involvement and cite where we can find any impacts, mitigation and coordination discussed within the environmental or other reference documents. If not, reference related analysis. And provide the docs, if they haven't yet been provided.

 <div data-bbox="207 646 571 877">  <p>Office of Bridge Programs</p> <ul style="list-style-type: none"> BRIDGE PROGRAMS HOME ENVIRONMENTAL WEBSITES LAWS & REGULATIONS BRIDGE PERMITS BRIDGE CLEARANCES RADIO/TELEPHONE BRIDGE LIGHTING MANUAL PUBLIC NOTICE CHANGING BRIDGE OPERATIONS DISTRICT BRIDGE CONTRACTS CONTRACT ADMINISTRATION E-READING ROOM AND PROJECTS EMAIL SUBSCRIPTION <div data-bbox="337 714 571 877"> <p>Bridge Permits</p> <p>The following has been prepared to assist Federal, State and local agencies, as well as members of the general public, when applying for a Coast Guard permit to construct a new bridge or roadway, or reconstruct or modify an existing bridge or roadway, across the navigable waters of the United States:</p> <ul style="list-style-type: none"> Coast Guard Bridge Permitting Overview Bridge Permit Application Guide (BPMG) BPMG Application Template (downloadable word document) Typical Sheet (downloadable word document) Permit Examination Checklist (2/15/02 & 1/18/12) (downloadable word document) 2011 Rule Book: Instructions for Environmental Reviews for Transportation and Other Off-Structure Projects MOU between USCG, FHWA, FTA, FDOT to Coordinate and Improve Bridge Permitting and Permitting </div> </div>	<p>20. <u>RESERVED</u></p> <p>On 21 January 2025, President Trump signed Executive Order 14173 (Ending Illegal Discrimination and Restoring Merit-Based Opportunity). EO 14173 revoked EO 12898 (Federal Actions to Address Environmental Justice in Minority Populations and Low-Income Populations). As a result of the revocation and in alignment with DHS, environmental justice will not be considered as part of any USCG permit action.</p> <p>Additionally, President Trump signed Executive Order (E.O.) 14148, Initial Rescissions of Harmful Executive Orders and Actions. E.O. 14148 rescinded the following Executive Orders E.O. 14008, Tackling the Climate Crisis at Home and Abroad; E.O. 14013, Rebuilding and Enhancing Programs to Resettle Refugees and Planning for the Impact of Climate Change on Migration; E.O. 14027, Establishment of the Climate Change Support Office; E.O. 14030, Climate-Related Financial Risk; and E.O. 13990, Protecting Public Health and the Environment and Restoring Science to Tackle the Climate Crisis. In alignment with the rescission of the Executive Orders listed, the USCG has eliminated the use of climate change terminology in its application.</p>

We officially conclude our traipse through the who, what, where, when, how, and what more with this... Change is constant and, these days, it even seems accelerated. We endeavor to remain in alignment with lawful administration mandates and Executive Orders. We're also available to answer your questions. You might have noticed that this application does not reference climate change or environmental justice. We don't require that at this time. If this or other major aspects change in the future, we'll post the updated documents and templates to our website. We will also push nationwide announcements through our email subscription service. You can sign up through our website by selecting Email Subscription at the bottom of the main menu. Previous bulletins are available on in the E-Reading Room page.

	<h1>QUESTIONS??</h1> <p>Search terms: Coast Guard bridge permit application</p> <p>https://www.dco.uscg.mil/Office-of-Bridge-Programs/</p> 	 

Thank you for your time. Any remaining questions?